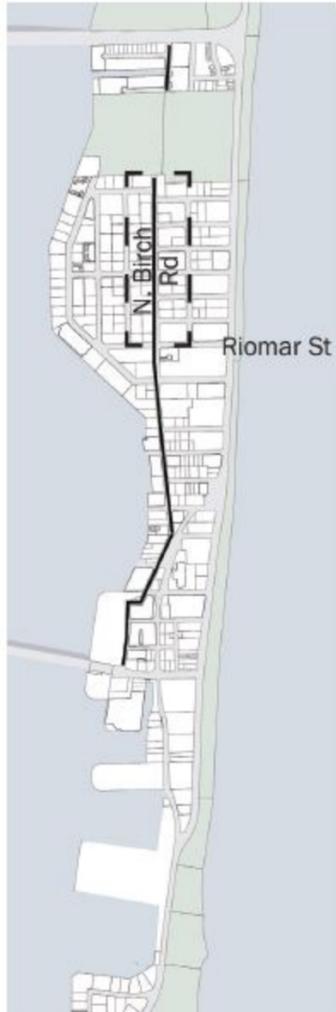


STREETSCAPE IMPROVEMENTS (NBRA)

N BIRCH ROAD (NORTH OF RIOMAR ST)



Key map



Image of N. Birch Rd. looking north at Riomar St



Image of N. Birch Rd. looking north at Viramar St.



Image of N. Birch Rd. north at Auramar St.

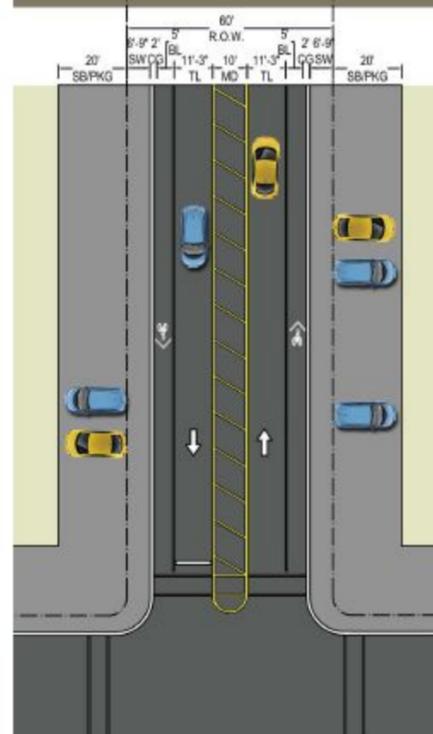
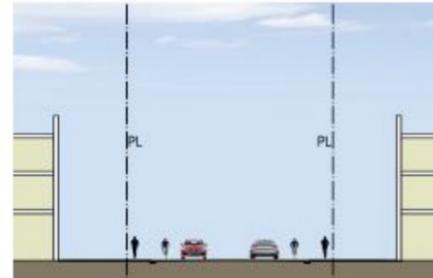
EXISTING



EXISTING CHALLENGES:

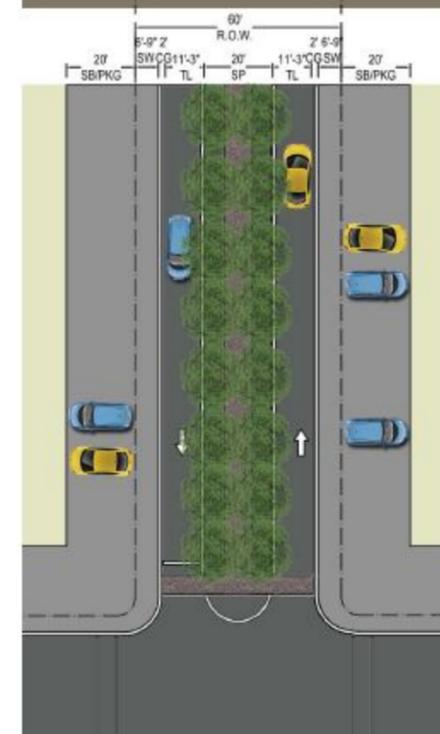
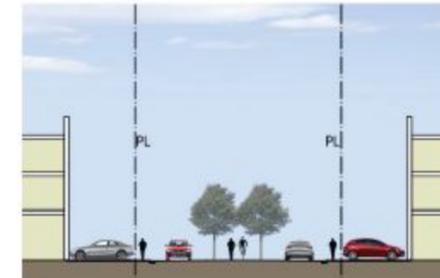
- Sidewalk widths are too narrow
- No landscaping along the street edge

PROPOSED



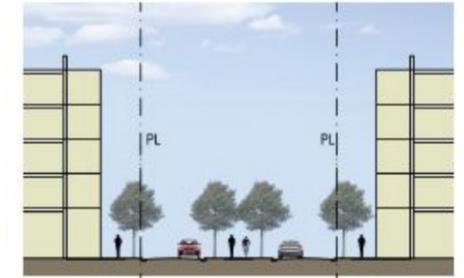
PHASE 1 RECOMMENDATIONS:

- Reduce the number of lanes from 4 to 2
- Stripe center turn lane
- Maintain existing curb



PHASE 2 RECOMMENDATIONS:

- Convert center turn lane to median with a shared-use path lined with street trees (greenway)
- Maintain existing curb



PHASE 3 RECOMMENDATIONS:

- Reduce travel lane width to 10'
- Provide parallel parking on both sides of the street
- Move existing curb
- 10' easement required for sidewalk and landscaping on both sides of the roadway as redevelopment occurs

Street Hierarchy Designation
- Secondary Street
Building Setback Required
- Minimum: 10 feet
- Maximum: 35 feet
Building Frontage Required
- 80%

Street Section Key:
BL Bike Lane
L Landscaping
PKG Parking
PL Property Line
SP Shared Use Path
R.O.W Right Of Way
SB Setback
SW Sidewalk
TL Travel Lane
MD Median

General Note:
Phase 1: Does not require a significant amount of capital infrastructure improvements and begins to show improvements on the ground.
Phase 2: Requires some capital infrastructure improvements.
Phase 3: Requires more significant capital infrastructure improvements and/or private full block redevelopment.
****All proposed improvements are subject to further policy decisions and prioritization.**



STREETSCAPE IMPROVEMENTS

EAST WEST TERTIARY ROADWAYS: BANYAN STREET TO BELMAR STREET



Key map



Image of Poinsettia St. looking west.



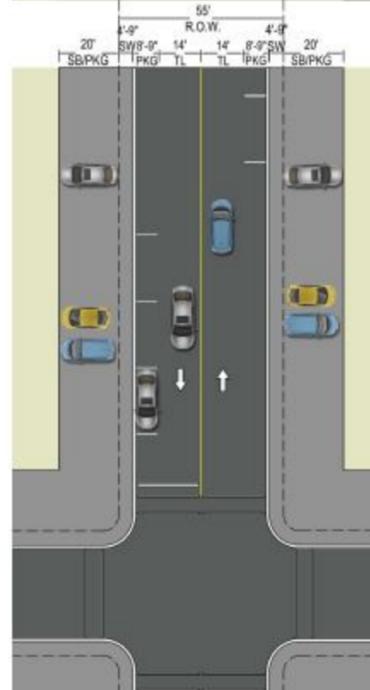
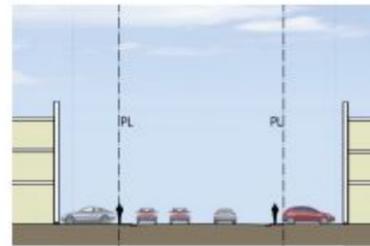
Image of Seville St. looking west.



Image of Auramar St. looking west.

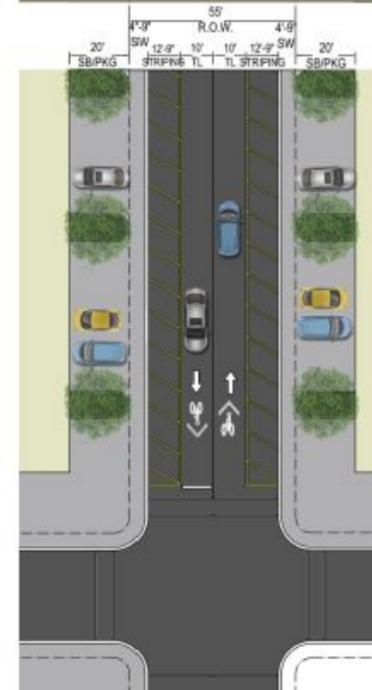
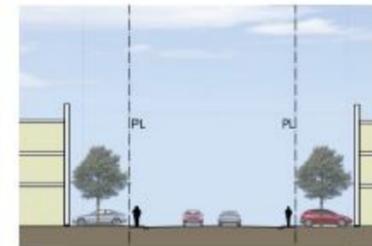
EXISTING

PROPOSED



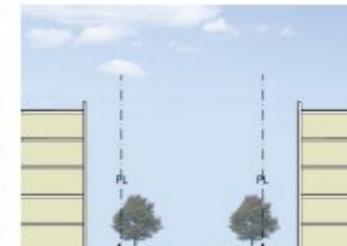
EXISTING CHALLENGES:

- Sidewalk widths are too narrow
- No landscaping along the street edge
- Travel lanes are too wide



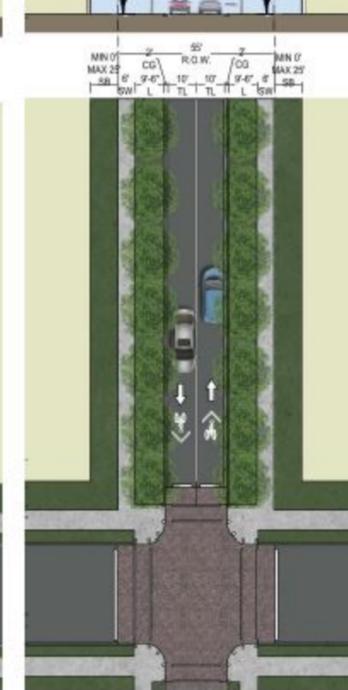
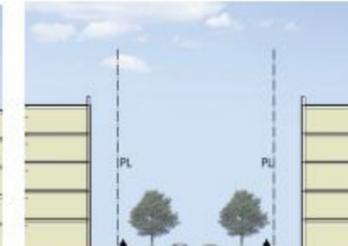
PHASE 1 RECOMMENDATIONS:

- Reduce travel lanes to 10'
- Maintain existing curb
- Stripe excess pavement area



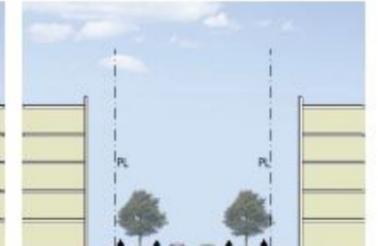
PHASE 2: OPTION A (PARKING ON BOTH SIDES) RECOMMENDATIONS:

- Move existing curb
- Provide a 5' sidewalk and a 5' landscape strip with shade trees along the street edge
- A 2.5' easement is required for sidewalk and landscaping on both sides of the street



PHASE 2: OPTION B (NO PARKING) RECOMMENDATIONS:

- Move existing curb
- Provide a 6' sidewalk and a 9'6" landscape strip with shade trees along the street edge



PHASE 2: OPTION C (BIKE LANES) RECOMMENDATIONS:

- Move existing curb
- Provide a 6' sidewalk, a 4'6" landscape strip with shade trees along the street edge, and a 5' bike lane on both sides of the street

Street Hierarchy Designation
- Tertiary Street
Building Setback Required
- Minimum: 2.5 feet (option 1); 0 feet (option 2)
- Maximum: 27.5 feet (option 1); 25 feet (option 2)
Building Frontage Required
- 70%

Street Section Key:
BL Bike Lane
L Landscaping
PKG Parking
PL Property Line
SP Shared Use Path
R.O.W Right Of Way
SB Setback
SW Sidewalk
TL Travel Lane
MD Median

General Note:
Phase 1: Does not require a significant amount of capital infrastructure improvements and begins to show improvements on the ground.
Phase 2: Requires more significant capital infrastructure improvements and/or private full block redevelopment.
****All proposed improvements are subject to further policy decisions and prioritization.**



STREETSCAPE IMPROVEMENTS (NBRA)

BAYSHORE DRIVE, ANTIOCH AVENUE AND ORTON AVENUE



Key map



Image of Bayshore Dr. looking north.

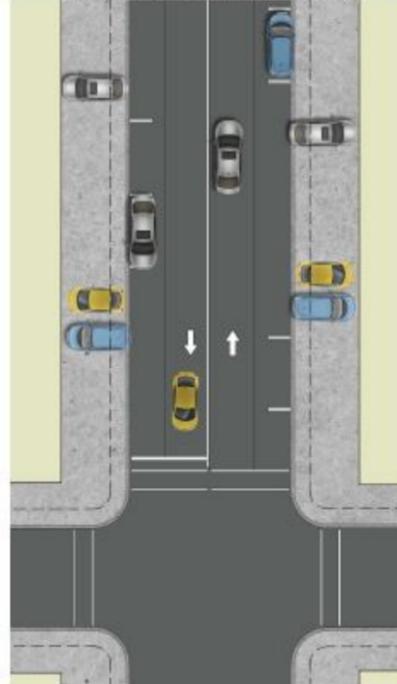
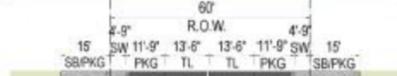
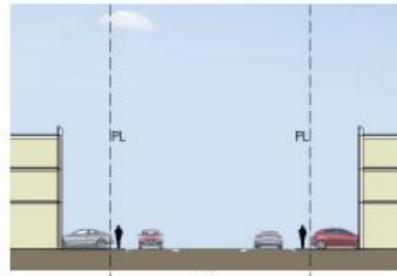


Image of Antioch Ave. looking north.



Image of Orton Ave. looking north

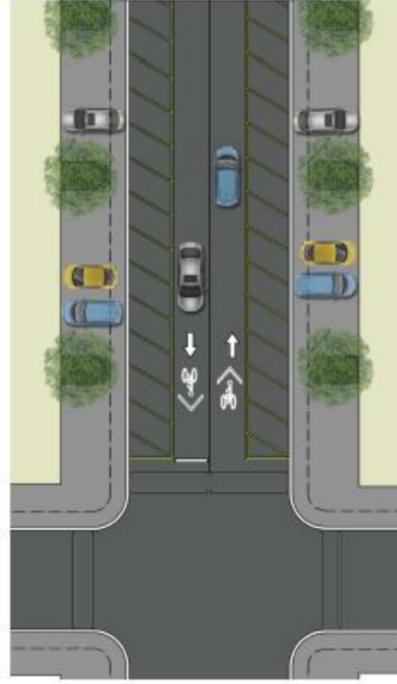
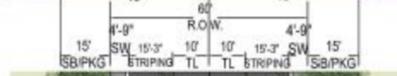
EXISTING



EXISTING CHALLENGES:

- Sidewalk widths are too narrow and occur behind back-out parking
- No landscaping along the street edge
- Travel lanes are too wide
- Parallel parking along street edge in conflict with existing back-out parking

PROPOSED



PHASE 1 RECOMMENDATIONS:

- Reduce travel lanes to 10'
- Maintain existing curbs
- Stripe excess pavement area



PHASE 2 RECOMMENDATIONS:

- Reduce R.O.W to 24'
- Move backout parking along edge of pavement
- A 15' easement is required for the sidewalk and landscaping on both sides of the roadway
- A landscape strip, a minimum of 5 feet wide, should be provided on each side of the sidewalk with shade trees on the side between the parking and the sidewalk

Notes:
R.O.W. to be vacated for private parking and 15' easements provided for public pedestrian improvements and landscaping

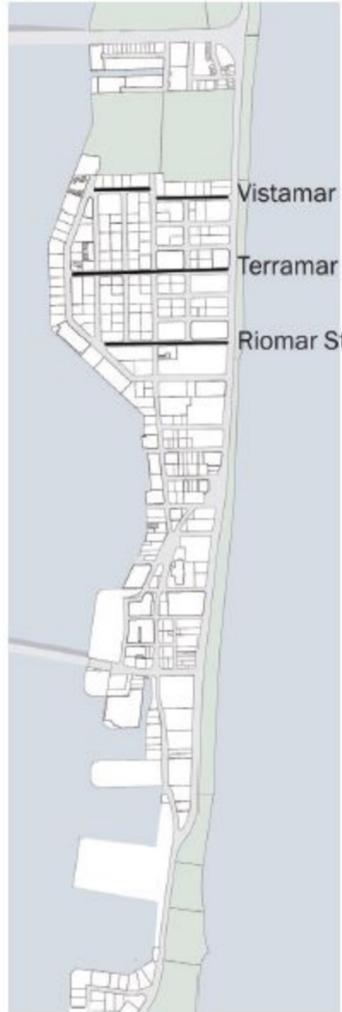
Street Hierarchy Designation	
-	Tertiary Street
Building Setback Required	
-	Minimum: 45 feet
-	Maximum: 60 feet
Building Frontage Required	
-	Bayshore Dr. 70% (east side), 40% west side
-	Antioch Ave. and Orton Ave: 70%

Street Section Key:		General Note: Phase 1: Does not require a significant amount of capital infrastructure improvements and begins to show improvements on the ground. Phase 2: Requires more significant capital infrastructure improvements and/or private full block redevelopment. **All proposed improvements are subject to further policy decisions and prioritization.
BL	Bike Lane	
L	Landscaping	
PKG	Parking	
PL	Property Line	
SP	Shared Use Path	
R.O.W	Right Of Way	
SB	Setback	
SW	Sidewalk	
TL	Travel Lane	
MD	Median	



STREETSCAPE IMPROVEMENTS (NBRA)

RIOMAR STREET, TERRAMAR STREET AND VISTAMAR STREET



Key map



Image of Riomar St. looking west.

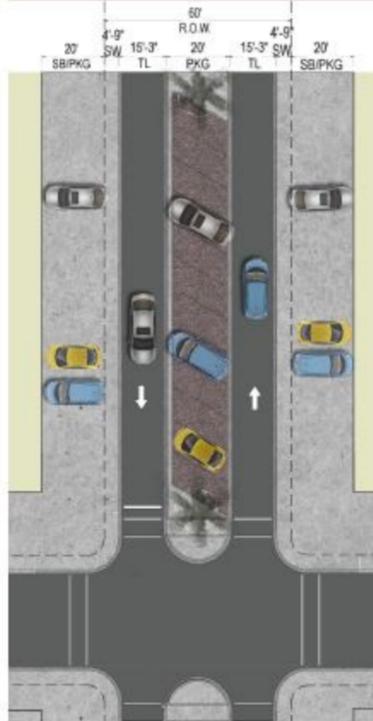


Image of Terramar St. looking west



Image of Vistamar St. looking west

EXISTING



EXISTING CHALLENGES:

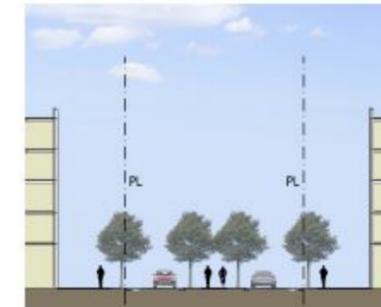
- Sidewalk widths are too narrow
- No landscaping along the street edge
- Travel lanes are too wide

PROPOSED



PHASE 1 RECOMMENDATIONS:

- Convert center parking area to median with a shared-use path lined with shade trees on both sides (greenway)
- Maintain existing curbs



PHASE 2 RECOMMENDATIONS:

- Move existing curb
- Narrow travel lanes to 10'
- Provide parallel parking on both sides of the street
- A 10' easement is required for sidewalks and landscaping on both sides of the roadway
- A minimum sidewalk width of 5 feet shall be provided on both sides of the street
- Tree grates a minimum of 5 feet by 5 feet or a landscape strip a minimum of 5 feet wide with street trees shall be provided along the street edge

Street Hierarchy Designation

- Secondary Street

Building Setback Required

- Minimum: 10 feet
- Maximum: 35 feet

Building Frontage Required

- 80%

Street Section Key:

- BL Bike Lane
- L Landscaping
- PKG Parking
- PL Property Line
- SP Shared Use Path
- R.O.W Right Of Way
- SB Setback
- SW Sidewalk
- TL Travel Lane
- MD Median

General Note:

Phase 1: Does not require a significant amount of capital infrastructure improvements and begins to show improvements on the ground.

Phase 2: Requires more significant capital infrastructure improvements and/or private full block redevelopment.

**All proposed improvements are subject to further policy decisions and prioritization.

STREETSCAPE IMPROVEMENTS (NBRA)

BREAKERS AVENUE (SOUTH OF BONNET HOUSE)



Key map



Image of Breakers Ave. looking north at Riomar St.

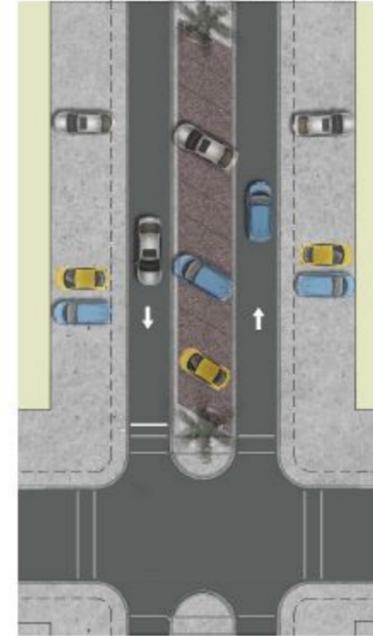


Image of Breakers Ave. looking north at Terramar St.



Image of Breakers Ave. looking north at Auramar St.

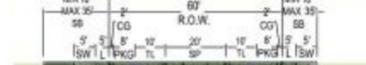
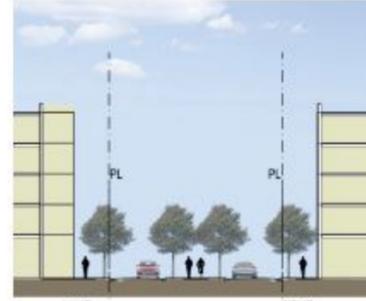
EXISTING



EXISTING CHALLENGES:

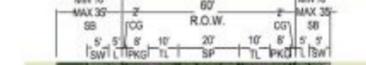
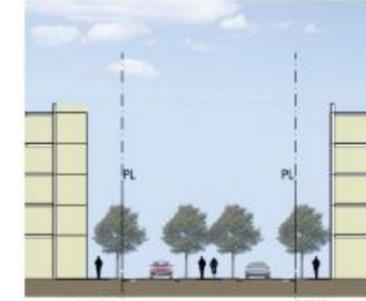
- Sidewalk widths are too narrow
- No landscaping along the street edge
- Travel lanes are too wide

PROPOSED



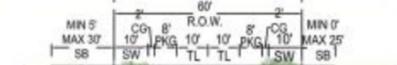
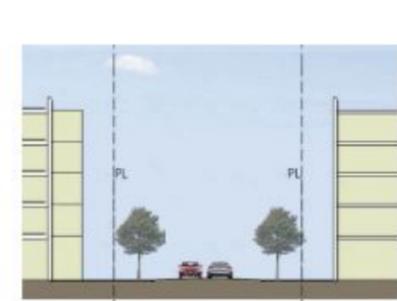
OPTION A RECOMMENDATIONS:

- Move existing curb
- Convert center parking area to median with a shared use path lined with shade trees on both sides (greenway)
- Reduce travel lanes to 10'
- Provide parallel parking
- A 10' easement is required for side walks and landscaping with shade trees on both sides of the roadway



OPTION B RECOMMENDATIONS:

- Same as Option A, with:
- Median is designed as a festival street with a flush streetscape condition
- Instead of landscape strips, 5'x5' planters with shade trees are utilized



OPTION C RECOMMENDATIONS:

- Move existing curb
- No median with a shared path (greenway)
- Incorporate Sharrow
- Reduce travel lane to 10'
- Provide parallel parking
- Provide a 5' sidewalk and 5'x5' planters with shade trees

<p>Street Hierarchy Designation</p> <ul style="list-style-type: none"> - Festival Street <p>Building Setback Required</p> <ul style="list-style-type: none"> - Minimum: 2.5 feet (option 1); 0 feet (option 2) - Maximum: 27.5 feet (option 1); 25 feet (option 2) <p>Building Frontage Required</p> <ul style="list-style-type: none"> - 70% 	<p>Street Section Key:</p> <ul style="list-style-type: none"> BL Bike Lane L Landscaping PKG Parking PL Property Line SP Shared Use Path R.O.W Right Of Way SB Setback SW Sidewalk TL Travel Lane MD Median 	<p>General Note:</p> <p>Option A, B & C : Requires significant capital infrastructure improvements and/or private full block redevelopment.</p> <p>**All proposed improvements are subject to further policy decisions and prioritization.</p>
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STREETSCAPE IMPROVEMENTS (SLA)

BREAKERS AVENUE (NORTH OF BONNET HOUSE)



Key map



Image of Breakers Ave. looking north at NE 9th St.

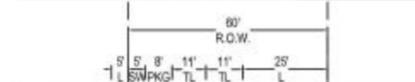
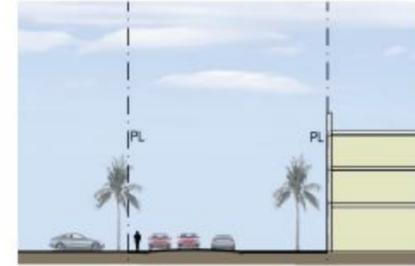


Image of Breakers Ave. looking north at NE 9th St.



Image of Breakers Ave. looking north at Sunrise Blvd.

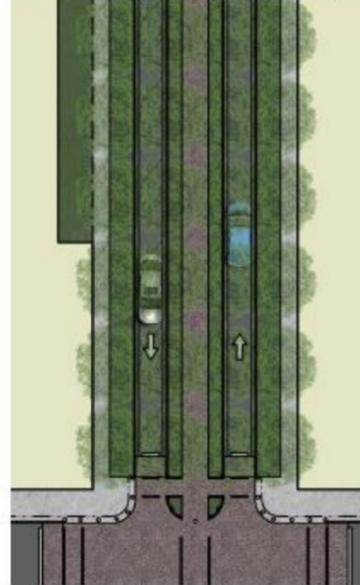
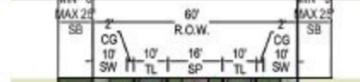
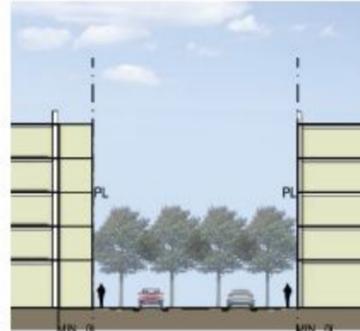
EXISTING



EXISTING CHALLENGES:

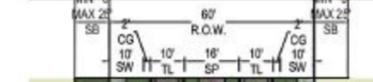
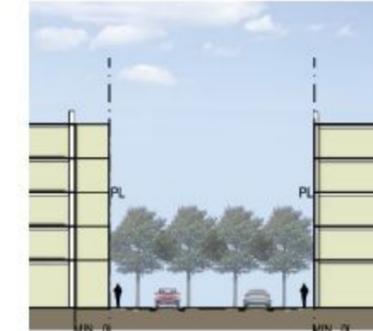
- Sidewalk widths are too narrow
- No sidewalk on the west side
- No landscaping along the eastside
- Development on the west side is fixed and no room for redevelopment or expansion of the R.O.W.
- Property on east side has redevelopment potential but no room for expansion of R.O.W.

PROPOSED



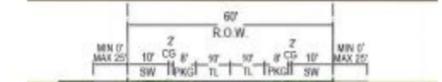
OPTION A RECOMMENDATIONS:

- Move existing curb
- Reduce travel lanes to 10'
- Provide a median with a shared use path that connects with the shared use path on Breakers Ave (south of Bonnet House), and establish a uniform streetscape for Breakers Ave
- Remove on-street parking on the east side
- Provide landscape strips with shade trees on both sides of the roadway
- Provide sidewalk along the west side



OPTION B RECOMMENDATIONS:

- Same as Option A
- Median is designed as a festival street with a flush streetscape condition
- Instead of landscape strips, 5'x5' planters are utilized



OPTION C RECOMMENDATIONS:

- Provide planters with shade trees on both sides of roadway
- Provide sidewalk along west side
- Provide parallel parking on both sides of the roadway
- Reduce travel lanes to 10'

Street Hierarchy Designation
- Festival Street
Building Setback Required
- Minimum: 0 feet
- Maximum: 25 feet
Building Frontage Required
- 80%

Street Section Key:
BL Bike Lane
L Landscaping
PKG Parking
PL Property Line
SP Shared Use Path
R.O.W Right Of Way
SB Setback
SW Sidewalk
TL Travel Lane
MD Median

General Note:
Option A, B & C : Requires significant capital infrastructure improvements.
 **All proposed improvements are subject to further policy decisions and prioritization.

STREETSCAPE IMPROVEMENTS (SLA)

SUNRISE LANE



Key map



Image of Sunrise Lane looking north at NE 9th St.

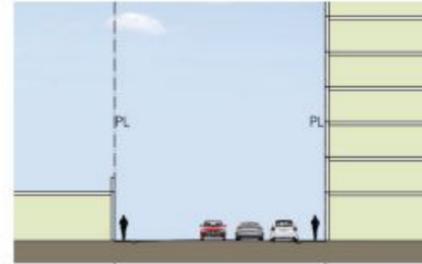


Image of Sunrise Lane looking north



Image of Sunrise Lane looking north at Sunrise Blvd

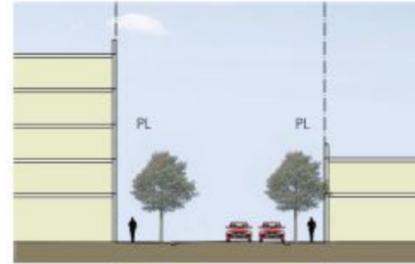
EXISTING



EXISTING CHALLENGES:

- No landscaping along the street edge

PROPOSED



PROPOSED RECOMMENDATIONS:

- Resolve parking deficiency for businesses (i.e. parking structure, off-site lease agreements, valet, etc.)
- When parking deficiency is resolved:
 - Convert to a one-way street
 - Change 90 degree back-out parking along commercial properties to angle-in parking
 - Provide parallel parking on the eastside
 - Widen sidewalk along the east side to 11'9"
 - Widen sidewalk along the west side to 10'
 - Tree grates a min. of 5'x5' with street trees shall be provided along the street edge

<p>Street Hierarchy Designation</p> <ul style="list-style-type: none"> - Festival Street <p>Building Setback Required</p> <ul style="list-style-type: none"> - Minimum: 0 feet - Maximum: 25 feet <p>Building Frontage Required</p> <ul style="list-style-type: none"> - 80% 	<p>Street Section Key:</p> <ul style="list-style-type: none"> BL Bike Lane L Landscaping PKG Parking PL Property Line SP Shared Use Path R.O.W Right Of Way SB Setback SW Sidewalk TL Travel Lane MD Median 	<p>General Note:</p> <p>Proposed: Requires a significant amount of capital infrastructure improvements.</p> <p>**All proposed improvements are subject to further policy decisions and prioritization.</p>
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STREETSCAPE IMPROVEMENTS (SLA)

NE 9TH STREET (EAST OF BREAKERS AVENUE)



Key map



Image of NE 9th Street looking west to A1A

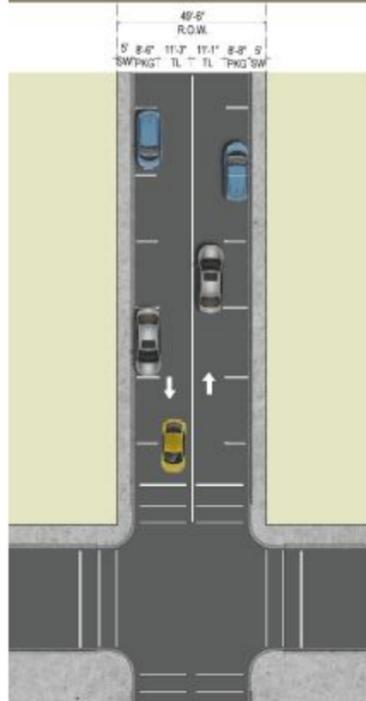
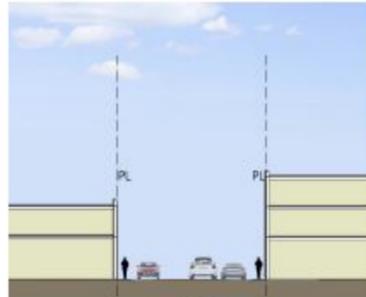


Image of NE 9th Street looking west



Image of NE 9th Street looking north at Breakers Ave.

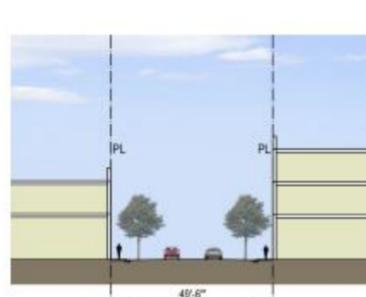
EXISTING



EXISTING CHALLENGES:

- Sidewalks are too narrow
- No landscaping along the street edge
- Travel lanes are too wide

PROPOSED



PROPOSED RECOMMENDATIONS:

- Resolve parking deficiency for businesses (i.e. parking structure, off-site lease agreements, valet, etc.)
- When parking deficiency is resolved:
 - Convert to a one-way street
 - Change 90 degree back-out parking along commercial properties to angle-in parking
 - Provide parallel parking on the east side

<p>Street Hierarchy Designation</p> <ul style="list-style-type: none"> - Festival Street <p>Building Setback Required</p> <ul style="list-style-type: none"> - Minimum: 0 feet - Maximum: 10 feet <p>Building Frontage Required</p> <ul style="list-style-type: none"> - 80% 	<p>Street Section Key:</p> <ul style="list-style-type: none"> BL Bike Lane L Landscaping PKG Parking PL Property Line SP Shared Use Path R.O.W Right Of Way SB Setback SW Sidewalk TL Travel Lane MD Median 	<p>General Note:</p> <p>Phase 1: Does not require a significant amount of capital infrastructure improvements and begins to show improvements on the ground.</p> <p>**All proposed improvements are subject to further policy decisions and prioritization.</p>
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STREETSCAPE IMPROVEMENTS (SLA)

N BIRCH ROAD (NORTH OF BONNET HOUSE)



Key map



Image of N Birch Rd. looking north at NE 9th St.

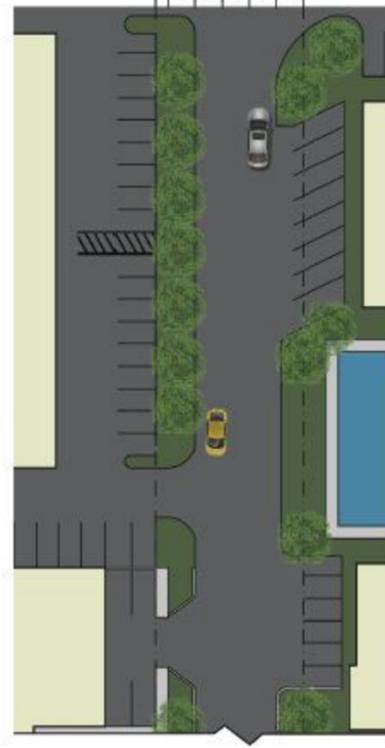


Image of N. Birch Rd. looking north between NE 9th St. and NE 9th Ct.



Image of N. Birch Rd. looking north at NE 9th Ct.

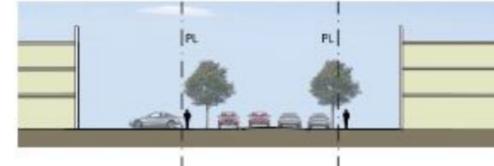
EXISTING



EXISTING CHALLENGES:

- Development on east side is fixed and no room for redevelopment or expansion of R.O.W.
- Backout parking conditions along west side
- Travel lanes are too wide
- Limited parking along west side
- No sidewalk along west side; discontinuous sidewalk along east
- excessive pavement

PROPOSED



PROPOSED RECOMMENDATIONS:

- Move existing curb
- Reduce travel lanes to 10'
- Replace backout parking on west side with parallel parking
- Provide parallel parking on east side
- Provide a continuous landscape strip and sidewalk on west side
- Extend sidewalk along east side

<p>Street Hierarchy Designation</p> <ul style="list-style-type: none"> - Secondary Street 	<p>Street Section Key:</p> <ul style="list-style-type: none"> BL Bike Lane L Landscaping PKG Parking PL Property Line SP Shared Use Path R.O.W Right Of Way SB Setback SW Sidewalk TL Travel Lane MD Median 	<p>General Note:</p> <p>Phase 1: Requires a significant amount of capital infrastructure improvements and/or private full block redevelopment.</p> <p>**All proposed improvements are subject to further policy decisions and prioritization.</p>
<p>Building Setback Required</p> <ul style="list-style-type: none"> - Minimum: 5 feet (west side), 0 feet (east side) - Maximum: 30 feet (west side), 25 feet (east side) 		
<p>Building Frontage Required</p> <ul style="list-style-type: none"> - N/A 		



STREETSCAPE IMPROVEMENTS (SLA)

NE 9TH COURT



Key map



Image of NE 9th Ct. looking west at N. Birch Rd.

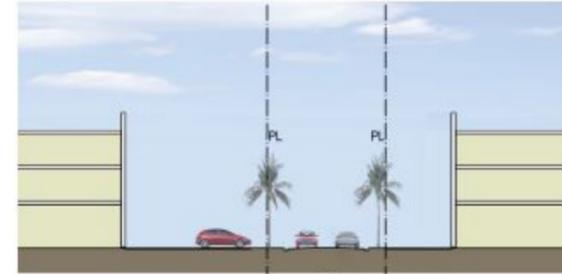


Image of NE 9th Ct. looking west



Image of NE 9th Ct. looking west

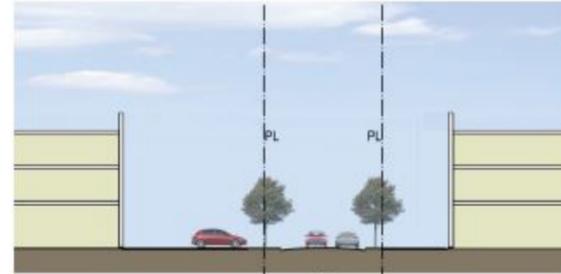
EXISTING



EXISTING CHALLENGES:

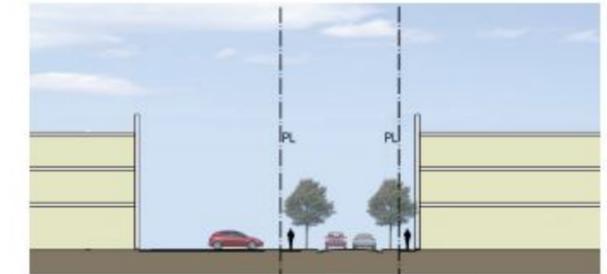
- No sidewalk provided
- Travel lanes are too wide

PROPOSED



PHASE 1: RECOMMENDATIONS:

- Narrow travel lanes to 10'
- Stripe excess pavement area



PHASE 2: RECOMMENDATIONS:

- 5' easement is required along the north side to provide a 5' sidewalk as redevelopment occurs
- A minimum sidewalk width of 5 feet, shall be provided on both sides of the road
- A landscape strip with shade trees, a min. of 5 feet wide shall be provided on both sides and shall be placed along the street edge

Street Hierarchy Designation - Tertiary Street	Street Section Key: BL Bike Lane L Landscaping PKG Parking PL Property Line SP Shared Use Path R.O.W Right Of Way SB Setback SW Sidewalk TL Travel Lane MD Median	General Note: Phase 1: Does not require a significant amount of capital infrastructure improvements and begins to show improvements on the ground. Phase 2: Requires more significant capital infrastructure improvements and/or private full block redevelopment. **All proposed improvements are subject to further policy decisions and prioritization.
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STREETSCAPE IMPROVEMENTS (SLA)

SUNRISE BOULEVARD



Key map



Image of Sunrise Blvd. looking west to A1A

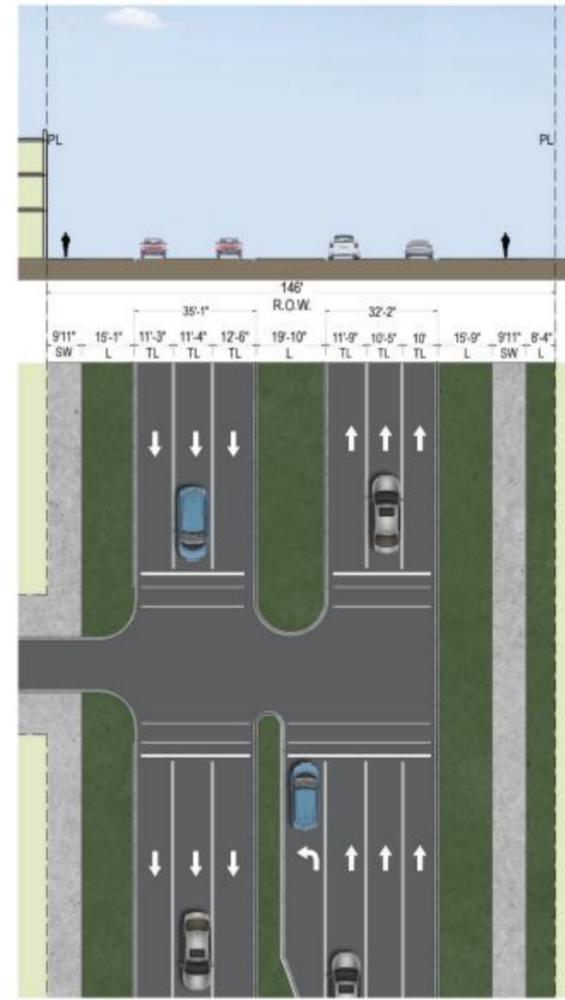


Image of Sunrise Blvd. looking west at Breakers Ave.



Image of Sunrise Blvd. looking west at N. Birch Rd.

EXISTING



EXISTING CHALLENGES:

- Travel lanes are too wide
- No bike lane is provided

PROPOSED



PROPOSED RECOMMENDATIONS:

- Narrow travel lanes to 10' on both sides
- Maintain existing curb on the south side
- A minimum sidewalk width of 9'-11" feet shall be provided on both sides of the road
- Move curb on the north side
- Incorporate a 5' bike lanes on both sides of the street
- A landscape strip with shade trees, a minimum of 15 feet wide on the south side and 12' wide on north side shall be provided along the street edge

<p>Street Hierarchy Designation</p> <ul style="list-style-type: none"> - Primary Street <p>Building Setback Required</p> <ul style="list-style-type: none"> - Minimum: 0 feet - Maximum: 25 feet <p>Building Frontage Required</p> <ul style="list-style-type: none"> - 70% 	<p>Street Section Key:</p> <ul style="list-style-type: none"> BL Bike Lane L Landscaping PKG Parking PL Property Line SP Shared Use Path R.O.W Right Of Way SB Setback SW Sidewalk TL Travel Lane MD Median 	<p>General Note:</p> <p>Phase1: Requires some capital infrastructure improvements.</p> <p>**All proposed improvements are subject to further policy decisions and prioritization.</p>
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