



DEVELOPMENT REVIEW COMMITTEE (DRC) COMMENT REPORT

MEETING DATE: April 13, 2021

PROPERTY OWNER: 616 SE 4th Avenue, LLC.

APPLICANT/AGENT: Joseph Handley, Craven Thompson & Associates

PROJECT NAME: 317 N Federal

CASE NUMBER: UDP-S21003

REQUEST: Site Plan Level II Review: 716 Residential Units, 8,713 Square Feet of Commercial Use in Downtown Regional Activity Center

LOCATION: 317 N Federal Highway

ZONING: Regional Activity Center - City Center District (RAC-CC)

LAND USE: Downtown Regional Activity Center (DRAC)

CASE PLANNER: Yvonne Redding



Case Number: UDP-S21003

CASE COMMENTS:

Please provide a response to the following:

1. Specify uses and occupancy classification per Chapter 3 of the FBC 2020.
2. Show provisions for either open or closed interior parking per FBC 406.5 or 406.6
3. Specify height and area compliance per Chapter 5 of the FBC 2020.
4. Provide building construction type designation per Chapter 6 of the FBC 2020. Depending on the building type wood cladding might not be allowed by the FBC.
5. Specify fire-resistance rating requirements based on building separation FBC Table 601 and 602.
6. Provide occupancy loads with compliant life safety egress design per Chapter 10 of the FBC 2020.
7. Indicate code compliant sprinkler system per FBC 2020.
8. Designate Fair Housing Provisions per FBC Accessibility volume.
9. Specify required number of exits based on travel distance, occupancy load, and use FBC 1006.
10. Differential settlement between the towers should be analyzed if the construction of the towers is being phased out.

GENERAL COMMENTS

The following comments are for informational purposes.

Please consider the following prior to submittal for Final DRC:

1. The Florida Building Code shall apply to the construction, alteration, movement, enlargement, replacement, repair, equipment, use and occupancy, location, maintenance, removal and demolition of every building or structure or any appurtenances connected or attached to such buildings or structures.
2. All projects must consider safeguards during the construction process. Florida Building Code Chapter 33 delineates various safeguards that may apply during the construction phase. All structures associated with the protection of pedestrians will require a separate permit. A licensed professional must sign and seal the plans and specifications.
3. The City of Fort Lauderdale is a participating municipality in the National Flood Insurance Program (NFIP). The requirements specific to the City of Fort Lauderdale can be found in

Chapter 14 - FLOODPLAIN MANAGEMENT of the Code of Ordinances and accessed at:

- a. https://www.municode.com/library/fl/fort_lauderdale/codes/code_of_ordinances?nodeId=COOR_CH14FLMA

Please consider the following prior to submittal for Building Permit:

1. On December 31st, 2021 the 7th Edition of the Florida Building Code was adopted. All work described in Section 101.2, of the Broward County Administrative portion of the Florida Building Code, will govern the administration and enforcement of the proposed work. Each building and or structure will require a separate permit. The following websites will assist in the design considerations:
 - a. <http://www.fortlauderdale.gov/departments/sustainable-development/building-services>
 - b. https://floridabuilding.org/bc/bc_default.aspx
 - c. <http://www.broward.org/codeappeals/pages/default.aspx>

General Guidelines Checklist is available upon request.



Case Number: UDP-S21003

DEDICATION OF RIGHTS-OF-WAY: Per ULDR Section 47-25.2.M.5, property shall be conveyed to the public by plat, deed or grant of easement as needed in accordance with the Broward County Trafficways Plan, the city's comprehensive plan, subdivision regulations and accepted applicable traffic engineering standards:

- a. Provide 8' Right-of-Way dedication or permanent Right-of-Way Easement along west side of N Federal Highway (coordinate with FDOT), to complete half of 120' Right-of-Way section per the most current Broward County Trafficways Plan; show / label delineation in the plans.
- b. Provide copy of Pre-application meeting memorandum with FDOT.
- c. Provide 25' corner chord Right-of-Way dedication or permanent Right-of-Way Easement on southwest corner of North Federal Highway & NE 4th Street intersection and northwest corner of North Federal Highway & NE 3th Street intersection (coordinate with FDOT) per ULDR Section 47-24.5.D.p; show / label delineation in the plans.
- d. Provide permanent Sidewalk Easement as appropriate along west side of North Federal Highway to accommodate portion of pedestrian clear path (coordinate required width with FDOT and TAM) that may be located beyond public Right-of-Way; show/label delineation in the plans.
- e. Provide permanent Sidewalk Easement as appropriate along south side of NE 4th Street to accommodate portion of pedestrian clear path (coordinate required width with TAM) that may be located beyond public Right-of-Way and/or Right-of-Way Easement dedication (per City's Downtown RAC Master Plan guidelines as appropriate); show / label delineation in the plans.
- f. Provide 10' x 15' (min.) permanent Utility Easement for any 4 Inch or larger water meter and/or the first private sanitary sewer manhole located within the proposed development (for City Maintenance access); show / label delineation in the plans as appropriate.

CASE COMMENTS:

Prior to Planning and Zoning Board Meeting sign-off, please provide updated plans and written response to the following review comments:

1. The corresponding Alley Vacation (Case UDP-EV21001) shall be approved by City Commission prior to Final DRC Sign-off. Depict the limits of the utility easement vacation on site plan.
2. The location of the existing property boundary line delineated in site plan and the corresponding ALTA/NSPS Land Title Survey along North Federal Highway is not consistent with the property line per the Broward County Property Appraiser's website. Please verify.
3. Any improvements proposed within a City public easement (i.e., 10' Utility Easement fronting North Federal Highway) will require a 'letter of no objection' from the City's Public Works Department. Please coordinate with Dronix Suarez at DSuarez@fortlauderdale.gov for obtaining the letter of objection. In addition, coordinate with FPL regarding the existing easements with the development.
4. Provide disposition of existing down guys located along NE 3rd St that may encroach within the driveway entrance/exit. In addition, ensure the concrete poles to remain along NE 4th Street does not conflict with the sidewalk clear path.
5. Coordinate with FDOT to confirm that proposed structures and on-site improvements (i.e. building terrace, building foundation, building overhang, etc.) are not be constructed within the 25' Corner Chord as applicable. If encroachments are proposed, provide correspondence from FDOT indicating the improvements are allowed.



6. Clearly indicate on plans the limits of construction and how the proposed improvements will transition into the existing (on-site and off-site).
7. Depict/ label proposed stop bar for vehicles exiting to NE 3rd Street from the driveway to west.
8. Per ULDR Section 47-2.2.Q, show and label sight triangles as appropriate for safe and adequate access of drivers and pedestrians, on the Site Plan, Landscape Plans, and Civil Plans: intersections of driveways with streets and alleys (10' measured from intersection point of pavement edges) and streets with streets (25' measured from intersection point of extended property lines).
 - a. Ensure the driveway sight triangles are measured from the intersection of the of the roadway edge of pavement (not sidewalk) and the driveway.
 - b. Per ULDR Section 47-19.1.E, no accessory use or structure shall be permitted within a sight triangle except as provided in ULDR Section 47-35.
 - c. Provide and label FDOT sight triangle (per the most current FDOT Design Standards) on the Site Plan, Landscape Plan, and Civil Plans at the southwest corner of NE 4th Street and North Federal Highway and the northwest corner of NE 3rd Street and North Federal Highway. Ensure the sight triangles meets the FDOT sight visibility requirements.
9. Proposed exterior building doors, loading zone doors, dumpster doors, etc. shall not open into the public Right-of-Way and/or permanently dedicated Right-of-Way Easements and Sidewalk Easements, adjacent sidewalk, ADA accessible path, or drive aisle areas; instead, consider recessing into building to enhance pedestrian safety; Verify if doors are proposed along NE 4th Street to access the FPL vault room.
10. Discuss dumpster enclosure/ trash pick-up access requirements with case planner and waste management. Clearly depict trash enclosure on site plan. Show truck turning movements in and out the proposed dumpster enclosure/ building as applicable. Ensure sufficient height clearance is provided within garage for truck access. Per ULDR Section 47-19.4.D.6, all dumpster enclosures and adjacent service access drive shall be placed on poured concrete, solid or perforated interlocking concrete block paving (ICB), or any existing hardened paving system.
11. Per ULDR Section 47-19.4.D.7, dumpsters serving restaurants require grease traps, oil / sand separators, a raised concrete slab, and a drain connecting to sanitary sewer. The elevation of either the raised concrete slab or the drain shall be set at or above the 100-year Broward County elevation to minimize inflow and infiltration into the City's sanitary sewer system.
12. Sheet A-305: Verify the location of the property line (including the sidewalk easement) to ensure there no conflicts within the sidewalk clear path and is consistent with the right-of-way boundaries shown on the site plan. Label vertical clearance above public access sidewalks along NE 4th Street and NE 3rd Street, if any building overhang is proposed.
13. Provide and label typical roadway cross-sections for the proposed development side of NE 3rd Street, NE 4th Street, and North Federal Highway at driveway access points, at on-street parallel parking lanes, and at landscape swale areas (i.e. between travel lane and sidewalk) as appropriate.
 - a. Confirm that the proposed sidewalk meets ADA guidelines.
 - b. Verify the walkway connection between the on-street parking area and the sidewalk along NE 3rd Street. It appears that stairs are being proposed.
14. Sheet C-6: Ensure ADA ramps on the Southwest corner of NE 4th Street and the Northwest corner of NE 3rd Street comply with FDOT Design Standard Index 522-002 Detectible Warning and Sidewalk Curb Ramps.



15. Label on Site Plan Data Table the required and proposed type of loading zone(s) required, per ULDR Section 47-20.2 Table 2 and Section 47-20.6; also label location of ADA van-accessible parking stalls. If applicable, show truck turning template circulation (label typical minimum centerline turning radius) entering and exiting the site as required for the proposed development. Turning geometries and loading zone design shall be in accordance with ULDR Section 47-20.6.
16. For all levels in the parking garage:
 - a. A min. 12 feet wide lane must be provided on both sides of the proposed column (or raise separator) within the parking garage ramp per ULDR Section 47-20.5.C.3.b.i.
 - b. Show and label dimensions for dead-end parking spaces as appropriate for vehicular turnaround. Per ULDR Section 47-20.5.C.4, dead-end parking areas shall be prohibited, except where the number of parking spaces in the dead end area is less than 21 and a turnaround area is provided which will accommodate a 2-point turn around by a standard passenger car (AASHTO "P" Design Vehicle) or where the number of parking spaces in the dead end is 10 or less.
17. Please provide reasonable assurances that the drainage system (including exfiltration trenches and catch basins) located beneath the parking garage structure will be able to be effectively operated and maintained. Please provide at a minimum:
 - a. Detailed information regarding the structural design of the parking garage and provide a certified geotechnical and structural engineering analysis to demonstrate that the proposed storage system will not undermine the structural components of the garage.
 - b. Design plans of the parking garage demonstrating that the structures are accessible by equipment required to maintain the system.
 - c. A detailed plan of how the system will be replaced in the event of failure.
18. Proposed trees shall be installed a min. 4 feet behind proposed curbs when adjacent to travel lanes and a min. 6 feet away from adjacent travel lanes when no curb is present.
19. Within adjacent City Right-of-Way, staging/storage will not be allowed, construction fence shall not encroach within intersection corner sight triangles, construction fence gates shall not swing into the public Right-of-Way, and any loaded Jib Crane radius shall not extend beyond private property boundaries. Any City Right-of-Way closure over 72 hours requires a Revocable License Agreement, processed by Property Right-Of-Way Committee (DRC) and approved by the City's Commission, prior to Right-of-Way permit issuance by the Department of Sustainable Development for Maintenance of Traffic.

Please provide a statement describing potential impacts to public right-of-way during construction of the development. This statement shall describe, but not limited to:

- Potential roadway and sidewalk closures, including phasing and duration.
- Use of pedestrian overhead canopy to maintain pedestrian travel.
- Impacts to public parking areas and parking arrangements for construction personnel.
- Crane operations
- Temporary occupancy of any City of Fort Lauderdale right-of-way needed for this project and the durations.
- Describe the construction impacts of the proposed design on the adjacent areas (right-of-way, properties, etc.) and the unique limitations of the site such as proximity to major roadways and waterways.

Prior to submitting Administrative Review (i.e. DRC Level I) application to request Revocable License Agreement, please contact Dennis Girisgen at 954-828-5123 or dgirisgen@fortlauderdale.gov to



- discuss proposed scope of closure within City Right-of-Way. Please note that the following scenarios will warrant a Revocable License:
- Any detours to a direction of vehicular and/or pedestrian travel for more than 72 hours continuously
 - Any enclosure of the public right-of-way with fencing, barriers, or other devices for any period of time.
 - Operation of any cranes in the right-of-way for more than 3 days
20. Discuss if pedestrian lighting is proposed; if not, discuss the possibility of the addition of pedestrian lighting along City Right-of-Way, which requires perpetual maintenance by the Applicant via a Maintenance Agreement executed with the City. Please contact the Case Planner for details to match the area.
21. Provide Maintenance Agreement Area Exhibit, which provides a visual representation of the area within the adjacent public Right-of-Way (adjacent to the proposed development) to be maintained in perpetuity by the developer. Label whether the adjacent Right-of-Way is FDOT, BCHCED, or City jurisdiction, as well as label all proposed improvements, including asphalt and other specialty paving, specialty sidewalks, landscaping, irrigation, lighting, curb and gutter etc. that will be maintained by the Applicant throughout the life of the improvements. Perpetual maintenance of newly constructed exfiltration trenches, inlets, etc. within adjacent City Right-of-Way will typically revert back to the City, upon successful inspection/acceptance by Public Works after 1-year warranty period.
22. Prepare service demand calculations for water & wastewater services and obtain a letter of service availability from the City's Public Works – Engineering Department in order to meet the City's adequacy requirements per ULDR Section 47-25.2 of the City's Code of Ordinances. Submit water and wastewater capacity availability request form and documents/ plans at <https://www.fortlauderdale.gov/departments/sustainable-development/building-services/engineering-forms-and-info/development-review-committee-service-demand-calculations-for-water-sewer-request-form>
23. Please email plans@fortlauderdale.gov to obtain copies of the City utility maps as applicable to the project location and show existing City utilities and easements (water, sewer, forcemain, and stormwater utilities) in proposed engineering plans (utility demolition, stormwater pollution prevention plan (SWPPP), and civil plans).
24. Label proposed water service connections and sewer lateral sizes and material on conceptual Water and Sewer Plan. Sanitary sewer clean-out must be provided at property line per City standards. Based on the City utility maps, the existing sewer main adjacent to this property is at a deeper elevation than assumed, verify elevations accordingly. Also provide disposition of existing services (i.e. water services and sewer laterals). Specifically, provide 10' x 15' utility easement and callout for referenced easement in plans for 8" DIP fire service located on SW corner of property and on 6" DIP residential water service on north side of property.
25. Conceptual Paving, Grading, and Drainage Plan:
- a. Provide sufficient existing and proposed grades and information on conceptual Paving, Grading, and Drainage Plan and details to demonstrate how stormwater runoff will remain onsite (include typical cross-sections along all property lines as appropriate, typical lot grading for the proposed single family homes within the development, and depict how the new stormwater system will connect to the existing on-site drainage system), and how the proposed project improvements (i.e. on-street parking, sidewalks, etc.) will not adversely impact the adjacent Right-of-Way, properties and waterways. Show location of building roof



drains, and their proposed connection(s) to the on-site drainage system. Label existing City storm manholes/inlets using the labels provided in the figure at the end of these notes.

- b. Drainage mitigation shall be required for any impacts within the adjacent City Right-of-Way such as increased runoff or reduction of existing storage/treatment due to proposed improvements, in accordance with the City of Fort Lauderdale Comprehensive Plan (Ordinance C-15-08), Objective 4.1 under Infrastructure elements. Engineer of Record (EOR) shall evaluate the adjacent City roadway system capacity and demonstrate that the proposed improvements will not negatively impact the City's existing drainage system and provide recommendations in compliance with the City's Comprehensive Plan (i.e. meets or exceeds the 10-year/1-day storm event drainage criteria). Applicant shall be responsible for maintenance of these proposed storm drain infrastructure improvements located within City Right-of-Way during a 1-year warranty period, until accepted by the City's Public Works Department. Coordinate with FDOT for mitigation of additional runoff along North Federal Highway.
26. Provide storm runoff calculations, signed and sealed by a Florida registered professional engineer, which demonstrate how minimum criteria regarding adjacent street crown, water quality and finished floor elevations are met, as well as how the 25-year, 3-day and the 100-year, 3-day storm events are maintained on site with zero discharge to Right-of-Way and adjacent properties (Pre vs. Post analysis will only be allowed in areas that are not flood-prone). Please also comply with the City of Fort Lauderdale Comprehensive Plan (Ordinance C-08-18), Objective 4.1 under Infrastructure Elements, and be advised that effective 7/1/2017, all projects must comply with the Broward County 'Future Conditions Average Wet Season Groundwater Level' map.
27. If dewatering activities are anticipated, a notarized City dewatering affidavit shall be filed at City's building department when submitting a demolition or foundation permit along with any applicable dewatering permit form regulatory agencies such as the South Florida Water Management District or Broward County Department of Environmental Protection.
28. Exfiltration Trenches:
 - a. Provide exfiltration trench for all on-street parking areas (i.e. within City Right-of-Way adjacent to the proposed development) and corresponding drainage calculations.
 - b. Provide drainage inlet (per City standard details and specifications) on each end of exfiltration trench located within City Right-of-Way.
29. Show and label all existing and proposed utilities (utility type, material and size) on civil and landscaping plans for potential conflict, **especially existing storm drain pipe within adjacent NW 6th Ave and existing 6 inch CIP water main within adjacent NW 7th Avenue Right-of-Way (per City Utility Atlas Maps)**. A min. 5 feet and 10 feet horizontal clearance horizontal separation is required between city utilities infrastructure and proposed small and large trees, respectively (including proposed water and sewer services to the development). Any new trees (located within or adjacent to City Right-of-Way) should be placed with sufficient horizontal and vertical distances (per City, County, and State guidelines) to / from City's public infrastructure, including stormwater assets, to allow for continued Public Works maintenance without obstruction. If this cannot be accommodated due to field conditions, then the developer shall relocate the existing City's public infrastructure to resolve the conflict(s) and to comply with City's, County's & State's engineering standards/permits/policies. Ensure separation is provided and include a note regarding horizontal clearance requirement on the landscape plans.
30. For Engineering General Advisory DRC Information, please visit our website at <https://www.fortlauderdale.gov/home/showdocument?id=30249>



31. Additional comments may be forthcoming at the DRC meeting and once additional/ revised information is provided on plans.

Case Number: UDP-S21003

CASE COMMENTS:

Please provide a response to the following:

1. Section 47-13.2-.H. RAC Streetscape Design. This section talks about complying with the Chapter 4 Design Guidelines.
 - a. Overhead utilities to be relocated underground. Please relocate overhead utilities along NE 4th Street including those that cross over Federal HWY.
 - b. Along NE 3rd Street there are underground utilities that conflict with required streetscape. Section 47-13.20.H. also talks about placing the structure up to 7 feet from the build to line to allow shade tree street trees to be provided. Please investigate a wider streetscape area to alleviate the conflicts between the required streetscape and the underground utilities. Or please investigate shifting the underground utilities to allow the horizontal clearances required for the streetscape as to the Design Guidelines. Along NE 4th Street please demonstrate that the required horizontal clearance to the structure is being provided for shade tree street trees.
 - c. The city looks for a continuous shade canopy along the public realm of the sidewalk with the palm trees defining the corners of a street and street. Along NE 3rd Street and NE 4th Street please propose shade trees with the palm trees at the corners.
2. There may be a comment from city staff as to the two parallel on-street parking stalls at the east end of NE 3rd Street. These two spaces may be required to be removed, please propose appropriate landscape materials in their place.
3. While on-street parking is encouraged, the Design Guidelines indicate the parking lane be broken up by occasional planted bulb-outs. Please provide a bulb-out and tree along NE 3rd Street in the center of the parking row.
4. A minimum separation of 6 feet is required between the tree trunk and travel lane when curb and gutter DO NOT exist, and a minimum separation of 4 feet is required between the tree trunk and travel lane when curb and gutter DO exist. Illustrate this clearance.
5. Illustrate and label the horizontal clearance from tree trunk to edge of utility on the landscape plan. Landscaping must provide a minimum horizontal clearance of 5 feet for small trees and palms, and a minimum of 10 feet for large trees and palms from underground utilities. Provide a cross section detail to illustrate this clearance.
6. In regard to proper horizontal distance from TREES to existing or proposed STORM pipes and exfiltration trenches, PW STW OPS standard review note states as follows:

"The edge of any City's existing storm-water assets (pipes, exfiltration trenches, structures, or other) shall be located at 5' minimum (7' preferred) horizontal clearance from any proposed tree's root system and with appropriate root barriers per City's landscaping regulations. "

The 5' min, 7' preferred hor. requirement should be measured from the outer edge of the storm pipe or exfiltration trench to the radius of the tree root system—not from the center of a tree to the center of a pipe or other.



7. Utilities and site amenities such as walkways, flagpoles, transformers, fire hydrants, sewer and water supply lines, trash enclosures, and similar items located on the site shall not be placed adjacent to, in, or under required tree planting areas, as per ULDR Section 47-21.12. Confirm with civil, site and life safety plans that utilities and site amenities are not causing conflicts with proposed landscaping. Where conflicts exist, please shift the utility and/or site amenities to allow for required landscape installation.
8. It was indicated by city staff that a dedication may be required along Federal HWY from FDOT. Please indicate any changes such as the possibility of additional landscape materials to be proposed along Federal HWY.
9. Street trees adjacent to the sidewalk along Federal HWY, please provide Structural soil or product designed for root growth under paved areas.
10. Design Guidelines suggest the overall size of the street trees to be 20-22 feet tall, minimum 8 feet spread with a minimum 6 feet canopy clearance. Please investigate an increase of size to the street trees. Street trees within the public realm require a 7 feet canopy clearance those encroaching within the sight triangle require a minimum of 8 feet.
11. Please provide an overlay sheet demonstrating how the open space and area in landscaping is being provided.
12. For specimen size trees, provide ISA Certified Arborist report for specimen trees, as per ULDR 47-21.15. This report is to be on ISA Certified Arborist business letterhead with contact information and ISA Certification number clearly stated. This report would include tree survey with numbered trees, a corresponding table which includes tree number, botanical name and common name, trunk diameter at breast height, clear trunk for palms, condition percentage etc.; and a written assessment of existing tree characteristics. This information is required to calculate equivalent value mitigation.
13. Please have a certified ISA Arborist provide the information as to the existing trees including the condition ratings that will be required for mitigation purposes.
14. Sight triangles located at the intersection of a local street or driveway with a right-of-way under County, State or Federal jurisdiction, may be subject to the sight visibility requirements of those jurisdictions, as per ULDR 47-2.2.Q. Approval from jurisdiction for landscape installation in Right Of Way, preferred prior to final DRC sign off. If jurisdiction approval is obtained after DRC final sign off and the plans require any change, Administrative Review will be required prior to permit review.
15. Additional comments may be forthcoming prior to final DRC sign-off.

GENERAL COMMENTS

The following comments are for informational purposes.

Please consider the following prior to submittal for Building Permit:

1. A separate sub-permit application for Tree Removal, Relocation, and General Landscaping for site is required at time of master permit submittal. These are Landscape and Tree permit application documents different than the Broward County standard applications. Please not at this time of DRC submittal.
2. Proposed landscaping work in the City's right of way requires engineering permit and approval (GLandscape permit). This approval requires documents to be submitted for Engineering review. Note that Landscape will not approve permit review without these Engineering documents being submitted.
3. Provide separate Plumbing sub permit application for irrigation. Irrigation plans are required at time of Building permit submittal. Plans are to be in compliance with ULDR 47-21.6.A.11 and 47-21.10. Note that planting areas are to be irrigated on a separate zone than the turf areas so that once the plants are established, that particular zone can be shut off based on the season. The overall goal is to decrease water use through irrigation. Plant material must be grouped together based on watering needs (hydrozone) and turf areas must be limited and/or consolidated to less than 50% of the landscaped area. Illustrate hydrozones on planting plan and include calculations in table.



Case Number: UDP-S21003

CASE COMMENTS:

Please provide a response to the following:

1. Entry doors should be solid, impact-resistant or metal.
2. Residential units entry doors should be equipped with a quality secondary deadbolt locking system and have a 180-degree peephole or viewport for security.
3. All glazing should be impact resistant.
4. All elevator lobbies and/or elevators should be access controlled.
5. Ground-level stair doors should be egress only or access controlled. Stair doors into floors should be access controlled.
6. The site should be equipped with a comprehensive CCTV system that can retrieve an identifiable image of an individual on-site. The system should cover all entry-exit points, parking area, common areas, storage areas and any other area of concern on site.
7. The parking area resident's levels should be equipped with vehicular access control systems to prevent unauthorized intrusion or access to resident's vehicles.
8. Light reflective paint should be used in the parking garage to increase visibility and safety.
9. Easily identifiable emergency communication devices should be available at the pool areas, common areas and the parking garage.
10. The pool area should be equipped with a childproof access control feature to prevent unsupervised children access to the pool.
11. Lighting and landscaping should follow C.P.T.E.D. guidelines.

GENERAL COMMENTS

It is highly recommended that the managing company make arrangements for private security during construction.

Please submit responses in writing prior to DRC sign off.



Case Number: UDP-S21003

CASE COMMENTS:

Please provide a response to the following:

1. Garbage, Recycling and Bulk Trash shall be provided.
2. Recycling reduces the amount of trash your business creates and it is the best way to reduce monthly waste disposal costs and improve your company's bottom line.
3. Solid Waste Services shall be provided by a Private Contractor licensed by the City.
4. Service Days shall be: No restriction for Commercial collection. Service may not occur earlier than 7:00 am or later than 7:00 pm within 250 feet of residential.
5. Containers: must comply with 47-19.4
6. Solid Waste charges shall be collected in monthly lease with Sanitation account for property under one name (Commercial).
7. Solid Waste Collection shall be on private property container shall not be placed, stored or block the public street to perform service (large multifamily and commercial parcels).
8. Provide letter from chute company indicating make and model of proposed equipment and that it will meet the capacity needs of building.
9. Recommend trash chute accommodate recycling.
10. Draw equipment on plan to show it will fit in trash room.
11. Provide the turning radii to ensure garbage truck is able to service the west tower container.
12. Submit a Solid Waste Management Plan on your letterhead containing name of project, address, DRC case number, number of units if applicable, and indicate whether it is Pre or Final DRC.
 - o This letter is to be approved and signed off by the Sustainability Division, and should be attached to your drawings. Please email an electronic copy to smccutcheon@fortlauderdale.gov. Letter should include an analysis of the expected amounts of solid waste and recyclables that will be generated (if different from current capacity), and container requirements to meet proposed capacity.
 - o Community Inspections will reference this Solid Waste Plan for sanitation compliance issues at this location.

General Comments

The following comments are for informational purposes.

Please consider the following prior to submittal for Final DRC:

1. None



Case Number: UDP-S21003

CASE COMMENTS:

1. City staff is currently reviewing the submitted traffic study and comments will be forthcoming. Staff and consultant's review concerns shall be adequately resolved prior to gaining authorization for either the Planning & Zoning Board or City Commission hearings. Take into consideration that the review of this study will take about 4-6 weeks once all documents are received.
2. Proposed work on State-maintained roadways and intersection influence area must be approved by the Florida Department of Transportation (FDOT). Please provide the FDOT pre application access management letter.
3. Provide confirmation from FDOT on whether they will accept a right of way dedication or a sidewalk easement for the sidewalk being relocated on private property.
4. Show inbound and outbound stacking requirements from the property line to the first conflict point according to Section 47-20.5 General design of parking facilities for the driveway.
5. Include a table showing the proposed land uses, the floor area in square feet for each land use, the parking ratio, the number of parking spaces required by type, and the number of parking spaces proposed by type (standard, compact, handicapped, bicycle, loading, etc.). ULDR Sec. 47-20.2. - Parking and loading zone requirements.
6. All parking must be provided in accordance with design and construction standards of the ULDR Section 47-20.11. Show dimensions for: stall width, depth of stall 90 degrees to aisle, aisle width, width of stall parallel to aisle, module width, angle of parking stalls.
7. Illustrate clear sight triangle for the intersections and driveways.
8. Remove all parallel parking spaces on NE 3rd St that conflict with upstream and downstream driveway and intersection sight triangle requirements depicted in:
Table 212.11.2 Parking Restrictions for Driveways and Intersections of the FDOT FDM.
<https://fdotwww.blob.core.windows.net/sitefinity/docs/default-source/roadway/fdm/2021/2021fdm212intersections.pdf?>
9. Remove the parallel parking spaces on NE 3rd St between the sites proposed driveway and N Federal Hwy, these parking spaces are within the intersection influence area and must not conflict with Table 212.11.2 Parking Restrictions for Driveways and Intersections of the FDOT FDM.
<https://fdotwww.blob.core.windows.net/sitefinity/docs/default-source/roadway/fdm/2021/2021fdm212intersections.pdf?>
10. There appeared to be two different rectangle cells that represent columns proposed in the speed ramps, ramp corners, drive aisles, and in the way of accessing loading zones, please clarify which rectangular cells are columns and which aren't.



11. All internal circulation and queuing areas must be designed to accommodate the turning radii of the vehicles that will be using the site. Provide auto turn vehicular paths to depict how the ground floor site circulation will work.
12. If Valet operations are to be provided, the Valet operations must not use public right of way for sight circulation.
13. For information on the required vehicular reservoir requirement for valet parking, please look at the section in our city code listed below:
 - a. Sec. 47-20.17. - Vehicular reservoir spaces for drive-thru facilities. Valet parking facilities, 50 spaces or more are required to have a minimum 6 vehicular reservoir spaces.
 - b. A vehicular reservoir space ("VRS") is a space within a vehicular use area for the temporary stopping of a vehicle awaiting service as provided in this section. A VRS shall be twenty (20) feet long by ten (10) feet wide. A VRS shall be located in an area within a parking facility which is not used for any other vehicular use such as access, parking, site circulation or loading.
 - c. Each VRS shall be clearly defined on the site plan and shall be in a location that does not conflict or interfere with other traffic entering, using or leaving the site. Design configuration shall be such that there shall be no backing into the street permitted.
 - d. Reservoir spaces shall be measured from the front of the service position to the rear of the VRS.
14. Provide a minimum of 7 feet wide on **NE 3rd St & NE 4th St**. This minimum is in reference to clear, unobstructed pathways –Light poles, trees and landscaping is not to be included in this zone. Landscaping should be between the sidewalks and back of curb and a minimum of 4 feet in width.
15. The bike lane on NE 3rd St must be a minimum 5 feet wide.
16. The proposed locations for bicycle storage in the corners of the parking garage are unacceptable. When vehicles are parked adjacent to the proposed bicycle storage locations it is near impossible to get bicycles in and out.
17. Ensure all access points, sidewalks, walkways, and curb cuts are unobstructed and ADA accessible with appropriate slopes and detectible warning devices and indicate on the site plan. This includes all access to/from the site entrance. Add the dimension, clearances and slopes of the walkways.
18. Bicycle parking is strongly encouraged. Consult the APBP Bicycle Parking Guidelines, city of Fort Lauderdale Parking Standards and Broward County End-of-Trip Bicycle Facilities Guide. Look to provide the minimum long term and short-term bicycle parking based on requirements in the guide and include total counts on the site plan data sheet. Bicycle parking needs to be located on the site so that it is accessible to the public.
19. Additional comments may be provided upon further review.

GENERAL COMMENTS:

Please address comments below where applicable.



1. The City's Transportation & Mobility Department encourages the use of sustainable materials such as permeable pavement and electric car charge stations and installation of multimodal facilities such as bicycle pump stations and bike lockers.
2. Please note that any work within the City's right-of-way will require an MOT approved by Transportation and Mobility and Engineering for permitting. Any full closures of roadways, alleys, or sidewalks that are over 72 hours will require a Revocable License Agreement (RLA) with the City of Fort Lauderdale.

Case Number: UDP-S21003

CASE COMMENTS:

Please provide a response to the following:

- 1) The applicant is strongly encouraged to contact neighbors adjacent to, as well as condominium and neighborhood associations located within three hundred feet (300') of the development site, to advise of this proposal (a listing of officially-recognized associations is provided on the City's website: <https://www.fortlauderdale.gov/departments/city-manager-s-office/office-of-neighbor-support/neighborhood-associations> Map of neighborhood associations: <http://gis.fortlauderdale.gov>). Please provide acknowledgement and/or documentation of any public outreach.
- 2) The site is designated Downtown Regional Activity Center on the City's Future Land Use Map. The proposed use is permitted in this designation. This is not a determination on consistency with Comprehensive Plan goals, objectives, and policies.
- 3) Provide a Plat Determination Letter from Broward County Planning Council (BCPC) verifying whether the property needs to be platted or re-platted. If a plat or re-plat is not required, contact the Broward County, Development Management and Environmental Review Section, at (954) 357-8695 to ensure that the proposed project is consistent with the latest recorded plat restriction(s). If a plat note or non-vehicular access line (NVAL) amendment is needed, a separate application is required, which is reviewed administratively and can be found here: [Administrative Review Application](#)
- 4) Development applications requesting residential dwelling units in the D-RAC are subject to unit availability at the time of Development Review Committee (DRC) approval and remaining available units will be allocated at the time of site plan approval on a first come, first served basis. In the event RAC units are not available, an applicant may request flex units or in the event there are insufficient number of RAC units to allocate to an entire project, the unit allocation may be divided between D-RAC units and flex units. Be advised, the City is requesting a release of flex units from the BCPC, which is currently under review by BCPC staff. Staff will advise applicant on status of unit allocation during DRC approval process.
- 5) Pursuant to ULDR, Section 47-13.20.J, projects in the DRAC are subject to a 30-day request for review period by the City Commission or subject to City Commission approval for projects which deviate from the DRAC requirements. A separate application is required for City Commission and the applicant is responsible for all public notice requirements. Note: The City Clerk's office requires 48 hours' notice prior to a Commission meeting if a computer presentation is planned i.e. Power Point, to be provided on CD or flash drive and a copy submitted to the City Clerk, contact the project planner for more information (954-828-5265).
- 6) In regard to physical, communication, and radar obstructions, the FAA requires a review for interference by the proposed construction. Provide a letter from the FAA indicating that such review has been performed and the additional height above 175 feet is acceptable. FAA approval must be obtained prior to Pre-CC and/or Final-DRC sign-off unless otherwise deemed unnecessary by the City Airport Manager or designee.
- 7) The project does not meet certain Downtown RAC dimensional requirements and Downtown Master Plan (DMP) design intents as outlined in ULDR, Section 47-13.20 and Section 47-13.21, respectively. If the applicant is proposing to deviate from such, the applicant must identify alternative design solutions which demonstrate and maintain the overall design intent. Applications proposing alternate design solutions are subject to City Commission approval. Staff has commented below under the applicable category and has provided images to assist the applicant.



Streetscape Design

- a. The local streetscape design should reflect the cross section for local streets for NE 3rd Street and NE 4th Street with the following streetscape elements: travel lane, on-street parking, street trees, and minimum 7-foot wide clear sidewalk. Given the number of residential units and increase in pedestrian activity, staff recommends the clear sidewalk width be increased from 7 feet to 10 feet.
- b. NE 4th Street has existing overhead utilities, an existing FPL easement, proposed underground utilities, and overhead awnings that encroach into the FPL easement. Further analysis of the cross section design should be conducted to ensure all these elements do not create conflict with the streetscape design requirements including the undergrounding of overhead utilities.
- c. NE 3rd Street cross section proposes the 7-foot wide sidewalk ramping at various spots with stairs along the street frontage which does not provide for a harmonious pedestrian environment. Ramps and stairs should be relocated out of the public realm experience and internalized on the site.
- d. Federal Highway cross section proposes to slope the landscape area toward Federal Highway, which would not be permitted for stormwater collection – see Engineering Comments.
- e. The bike lane width has been reduced compared to existing width. Please match the correct width.
- f. The two on-street parking spaces on NE 3rd Street located closest to Federal Highway will need to be removed due to conflict with sight triangle.
- g. Note, ground floor doors should not encroach into the pedestrian clear path width.

Building Design

- h. As proposed, the project does not meet the setback along Federal Highway in order to frame the street with a consistent building line. The parking podium and the tower encroaches into the setback.
- i. The building streetwall length at 407 feet along NE 3rd Street exceeds the maximum 300 feet. Address accordingly.
- j. Parking podium design should be integrated into the overall building design where the podium and tower blend consistency in certain areas of the building elevations. In addition, there are large solid wall areas on the podium elevations that should be re-examined for other design solutions.
- k. Main pedestrian entrances should face the street including resident lobby entrances and retail space. As proposed, the project doesn't fully meet this design intent. The ground level floorplan contains a significant amount of lobby area, due to the two residential tower design. Consider combining the east and west lobby areas designating a significant focal/arrival entry design. This would result in a stronger building presence along the primary street, also allowing for additional retail/commercial space.
- l. Provide for continuous pedestrian shading devices at a minimum of 5 feet in depth along street frontages. Varying shade design is encouraged.
- m. Indicate podium step-back and tower separation dimensions on site data table and elevations from each property line.
- n. Provide required and proposed floorplate size; please coordinate plans and narratives to correlate.

Quality of Architecture

- o. As proposed, the top of the towers do not meet the design requirements for expressive and sculptural tower top design nor do the top of the towers add to the overall skyline composition. The design needs to be re-evaluated to meet this requirement.
- p. Parking podium should contain exceptional design solutions to screen the podium through various design techniques and materials.
- q. The building facades need additional layers of architectural composition with varying materials, recess lines, larger more expressive openings, and varying window sizes in order to meet the design intent in the DMP.



- 8) The proposed building is significantly larger in mass and scale than immediate surroundings. Mitigate building massing by incorporating stepbacks and transition the overall scale of the towers, paying particular attention to the transition across the residential areas across Federal Highway as well as the transition to the lower scale of projects in the Urban Neighborhood. The building towers should also contain a range of architectural styles with more vertical articulation in the building from ground level to the roof. Integrate more articulation by incorporating features such as large terraces, cut-outs open to the sky, and reconsider the large L-shape of the building. Refer to images below as design examples.



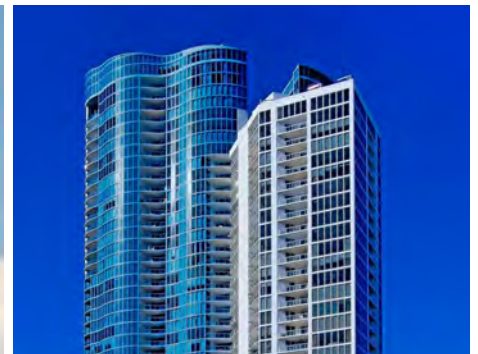
- 9) The proposed lower levels of the building, pedestrian scale levels, should be redesigned to incorporate double story height the full length of the building street frontages. Building corners should contain significant architectural features that are proportionate to the overall project scale with significant projections, change in material, and other design techniques. Refer to images below as design examples. In addition, provide pedestrian-level perspective renderings of project as viewed along the



street, and aerial oblique perspectives in context with adjacent properties and surroundings, from opposing views. Show clear and accurate 3-dimensional views in context with the surrounding area.



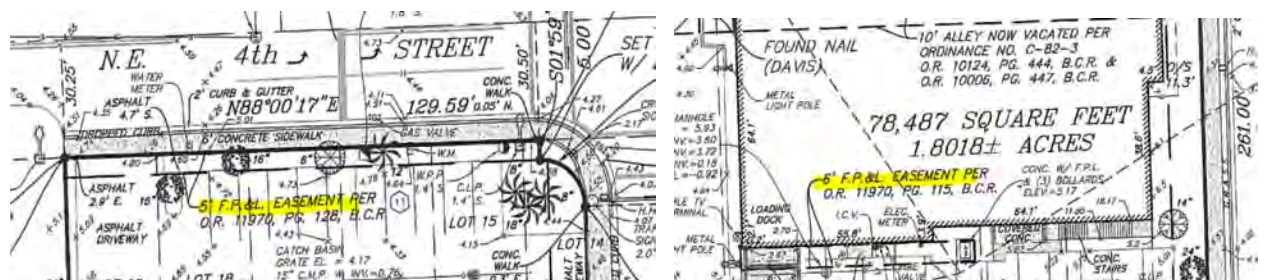
10) As previously stated, the towers top design does not meet the requirements of the DMP and require significant modification. Refer to images below.



11) Provide the following changes on site plan:

- a. Provide an outline of all adjacent buildings (indicate height in stories and feet).
- b. Parcel appears to be over-programmed with a poor balance of open space to build-out space; Provide breakdown of open space required versus proposed. There are two rows depicting ground level space and amenity decks. Ground level space should be separated from upper floor amenity decks/open space.

- c. The proposed artwork and outdoor seating depicted on the renderings should be shown on the site plan; the artwork and outdoor seating should not impede or obstruct the pedestrian clear path.
 - d. Resolve the sidewalk conflict between S-1 and A-200; they do not align in the same location.
 - e. The proposed drop-off porte cochere area needs to be redesigned as it does not provide for safe movement of vehicles and pedestrians with numerous conflicting movements and unsafe conditions. Furthermore, the space is significant in area compared to pedestrian plaza outdoor seating space given the program and overall size of the project.
- 12) Provide the following changes on elevations:
- a. Provide a building tower setbacks from Federal Highway similar to the "Dalmar Hotel" project to the south of the site, located at 299 N. Federal Highway.
 - b. The west elevation appears as a blank 9-story wall, with no articulation. The design should contain consistent and integrated architectural details similar to those facades facing the right-of-ways. Consider richer materials and more intensive details along this façade. Current development to the west is lower scale and will be impacted by the blank wall design.
 - c. Staff is unclear on the parking podium design with the 0850 interstitial level space, which should be labeled as the 0900 level and the podium height should be adjusted accordingly.
- 13) Provide additional information regarding proposed east and west tower pick up/drop off area located interior to the building. The circulation appears to be confusing and may need to be redesigned. Also, illustrate how this area will operate if the ground floor and parking levels are built in phases.
- 14) Pursuant to ULDR Section 47-19.2.Z, All rooftop mechanical equipment areas, stair and elevator towers should be designed as an integral part of the building volume and shall be required to be screened with material that matches the material used for the principal structure and shall be at least as high as six (6) inches above the top most surface of the roof mounted structure. Address the following:
- a. Provide detail drawing(s) with spot elevations of roof plan depicting adequate screening of roof mounted equipment.
 - b. Ensure the screening material is made of durable material and also meets the DMP tower design requirements.
 - c. Describe in detail the proposed use of the roof as there appears to be enclosed rooms on the roof and if access is intended now or in the future or if there is mechanical equipment located behind the clear glazed windows.
- 15) Provide night-time renderings of the project, include the north and west elevations facing the existing lower scale residential and retail uses existing today.
- 16) Provide information regarding relocation of Greyhound Bus Station terminal including documentation from Broward County regarding the relocated bus stop, signage and or weather protection devices.
- 17) The proposed project is located along the Florida Department of Transportation (FDOT) right-of-way; therefore, additional dedications may be required. If additional ROW is required to be dedicated, adjust the site plan accordingly. Coordinate FDOT pre-application letter referencing the proposed landscaping, right-of-way improvements, artwork, and furniture proposed with Engineering.
- 18) Provide information regarding the 5-foot and 6-foot FPL easements shown on the survey. If the easements are to be vacated, please submit the required applications.





- 19) Provide a phasing plan that depicts the areas under each phase and include a site data table showing each phase development use and how each phase will meet all the ULDR requirements, including parking, landscaping, open space, etc.
- 20) Provide a construction staging plan which includes anticipated hours of operation on site, debris mitigation plan, and map indicating where crane operations and employee and/or equipment parking and storage will be placed. If a temporary construction/sales trailer is needed for this project, provide the details and location of the trailer on a separate site plan, to avoid additional review in the future. Verify details and location with the Building Representative. A revocable license application and a traffic circulation plan may be required if the sidewalk or right-of-way requires to be closed at any time, which should be filed under a separate application and coordinated through the City's Maintenance of Traffic (MOT) process.
- 21) Pursuant to Section 47-22.4.C.8 provide a master sign plan detailing the following:
 - a. Location and orientation of all proposed signage;
 - b. Dimensions of each proposed sign (height, width, depth, etc.);
 - c. Proposed sign copy; and,
 - d. Proposed color and materials.

Please note any proposed signs will require a separate permit application.
- 22) The project is subject to the requirements of Broward County Public School Concurrency. The applicant will notify the School Board Superintendent or designee of this proposal. A written response from the School Board shall be provided by the applicant. Prior to application for final DRC approval, please provide confirmation from the School District that the residential development is exempt or vested from the requirements of public school concurrency, or a School Capacity Availability Determination (SCAD) letter that confirms that capacity is available, or if capacity is not available, that mitigation requirements have been satisfied.
- 23) Please provide total park impact fee amount due. Park impact fees are assessed and collected at time of permit per each new hotel room and dwelling unit type. An impact fee calculator can be found at: <https://www.fortlauderdale.gov/departments/sustainable-development/building-services/park-impact-fee-calculator>

General Comments

The following comments are for informational purposes. Please consider the following prior to submittal for Final DRC:

- 24) Please be advised that pursuant to State Statute, Section 166.033, development permits which require a quasi-judicial or public hearing decision, must be completed within 180 days unless an extension of time is mutually agreed upon between the City and the applicant.
- 25) Provide a written response to all DRC comments within 180 days.
- 26) Additional comments may be forthcoming at the DRC meeting.



DESIGN REVIEW TEAM (DRT) COMMENTS

Planning Area: Downtown Master Plan | Rev. 2/25/2021

BACKGROUND INFORMATION: The intent of the Design Review Team (DRT) is to assist applicants by identifying compliance with the applicable master plan design requirements in the form of a checklist. The checklist should be used as a guide in generating formal development applications for site plan submittal. Applicant should provide responses to each item in the DRT checklist upon submittal of formal development applications.

CASE NUMBER:	UDP-DRT21002	NOTES: <ul style="list-style-type: none"> Principles in bold are dimensional standards required by Code (ULDR) Section 47-13.20. Principles marked "yes" / "no" indicate if project meets intent of guideline N/A = Not applicable; more information needed means there is insufficient information to review.
PROJECT NAME:	317 N Federal	
PROJECT ADDRESS:	317 N. Federal Highway	
REVIEW DATE:	04/05/21	
CASE PLANNER:	Yvonne Redding	
CONTACT INFORMATION:	954-309-4910	

INSTRUCTIONS: Mark applicable column with "X". Add comments / design intent under Principle title.

PRINCIPLES OF STREET DESIGN		YES	NO	N/A	MORE INFORMATION NEEDED
S1	Maintain fine-grained street grid: discourage vacations. Comment: No vacations requested	X			
S2	Utilize Traffic Calming rather than blocking streets.			X	
S3	Maximize on-street parking except on major arterials.	X			
S4	Provide adequate bike lanes in a planned network (next to on street parking: 5 feet; next to travel lane: 4 feet). Comment: Adjust width shown on site plan				X
S5	Maximize street trees on all Downtown Streets.	X			
S6	Encourage location of primary row of street trees between sidewalk and street.	X			
S7	Maximum spacing for street trees: Palms -22 feet; Shade trees - 30 feet.	X			
S8	Minimum horizontal clearance (from building face) for trees: Palms – 6 feet; Shade trees – 12 feet	X			
S9	Encourage shade trees along streets, palm trees to mark intersections.	X			
S10	Eliminate County "corner chord" requirement not compatible with urban areas.			X	
S11	Encourage curb radius reduction to a preferred maximum 15 feet; 20 feet for major arterials.	X			
S12	Discourage curb cuts on "primary" streets.	X			
S13	Encourage reduced lane widths on all streets.			X	
S14	Encourage reduced design speeds on all RAC streets (15 - 40 mph).			X	
S15 *ULDR*	Encourage fixed Rights-of-Way and setbacks for all Downtown streets (to eliminate uncoordinated City setback and County easement requirements). Note: Downtown local streets have varying ROW's and section design may need to be flexible to respond to the specific right-of-way conditions. Comment: Provide cross section to ensure 35 feet to center line per the DTMP is met.				X
S16	Bury all power lines in the Downtown Area. Comment: Provide FPL correspondence regarding this.				X
PRINCIPLES OF BUILDING DESIGN		YES	NO	N/A	MORE INFORMATION NEEDED
B1	Framing the street: building "streetwall" should generally meet setback line (within a percentage).	X			
B2	Framing the street: encourage open space site requirements for use as pedestrian public space instead of unusable, leftover 'green perimeter'.			X	
B3 *ULDR*	Framing the street: minimum and maximum building 'streetwall' heights (see character area guidelines for specifics).	X			
B4 *ULDR*	Framing the street: Maintain maximum building 'streetwall' length of 300 feet		X		



	Comment: Length of streetwall along 3rd is over 300 feet.				
B5 *ULDR*	Maintain maximum Floorplate Area for towers based on character area				
B6 *ULDR*	Where towers are located on Primary (>60 feet wide) and Secondary (< or = 60 feet wide) Streets, the towers are encouraged to orient towards the Primary Street.	X			
B7 *ULDR*	Where towers are located on streets < or = 60 feet, increased setbacks from the 'shoulder' are encouraged at 30 feet to reduce the impact on the street.			X	
B8	Surface parking: discourage frontage and access along 'primary' street.	X			
B9	Parking garages: encourage access from secondary streets and alleys.	X			
B10	Encourage main pedestrian entrance to face street.		X		
B11	Maximize active uses and 'extroverted' ground floors with retail in strategic locations.	X			
B12	Encourage pedestrian shading devices of various types. Comment: Does not appear to provide shading devices along Federal Highway.		X		
B13	Encourage balconies and bay windows to animate residential building facades.	X			
B14	In residential buildings encourage individual entrances to ground floor units, particularly in the Urban Neighborhood Character Area			X	
B15	High rises to maximize active lower floor uses and pedestrian-oriented design at ground floor Comment: Ground floor design could be improved, by providing better connections, larger usable plaza areas, shading elements.	X	X		
B16	Building Design guidelines do not apply to Civic Buildings and Cultural Facilities.			X	
B17	Discourage development above right-of-way (air rights).			X	
B18	Mitigate light pollution.				X
B19	Mitigate noise pollution.				X
B20 *ULDR*	Vertical open space between towers on adjacent lots: Maintain 60-foot vertical open space between towers (30 Feet minimum on subject property if adjacent to abutting lot under separate ownership). In certain circumstances abutting property owners can coordinate tower placement as long as maintain 60-foot clearance.	X			
B21	Vertical open space between multiple towers on a single development site: no less than 60 feet apart.	X			
B22	Residential: Encourage minimum ground floor elevation of 2 feet above public sidewalk level for individual ground floor entrances to private units.			X	
B23	Avoid drive-thrus in the wrong places.			X	
B14	The Fifth Façade: Encourage green roofs as visual amenities that provide a combination of usable, landscaped spaces (recreation & open space benefits) and sustainable roof treatments (environmental benefits).	X			
QUALITY OF ARCHITECTURE		YES	NO	N/A	MORE INFORMATION NEEDED
Q1	Skyline Drama: Encourage towers to contribute to the overall skyline composition. Comment: Refer to DRC comment		X		
Q2	Expressive Tops: Encourage expressive tops for tall buildings above 37 stories in Near Downtown and Downtown Core. Comment: Refer to DRC comment		X		
Q3	Durability and Quality of Materials: Encourage high quality materials for the entire building, with special emphasis on detailing and durability for the first 2 floors Comment: Refer to DRC comment		X		
Q4	Respect for Historic Buildings			X	



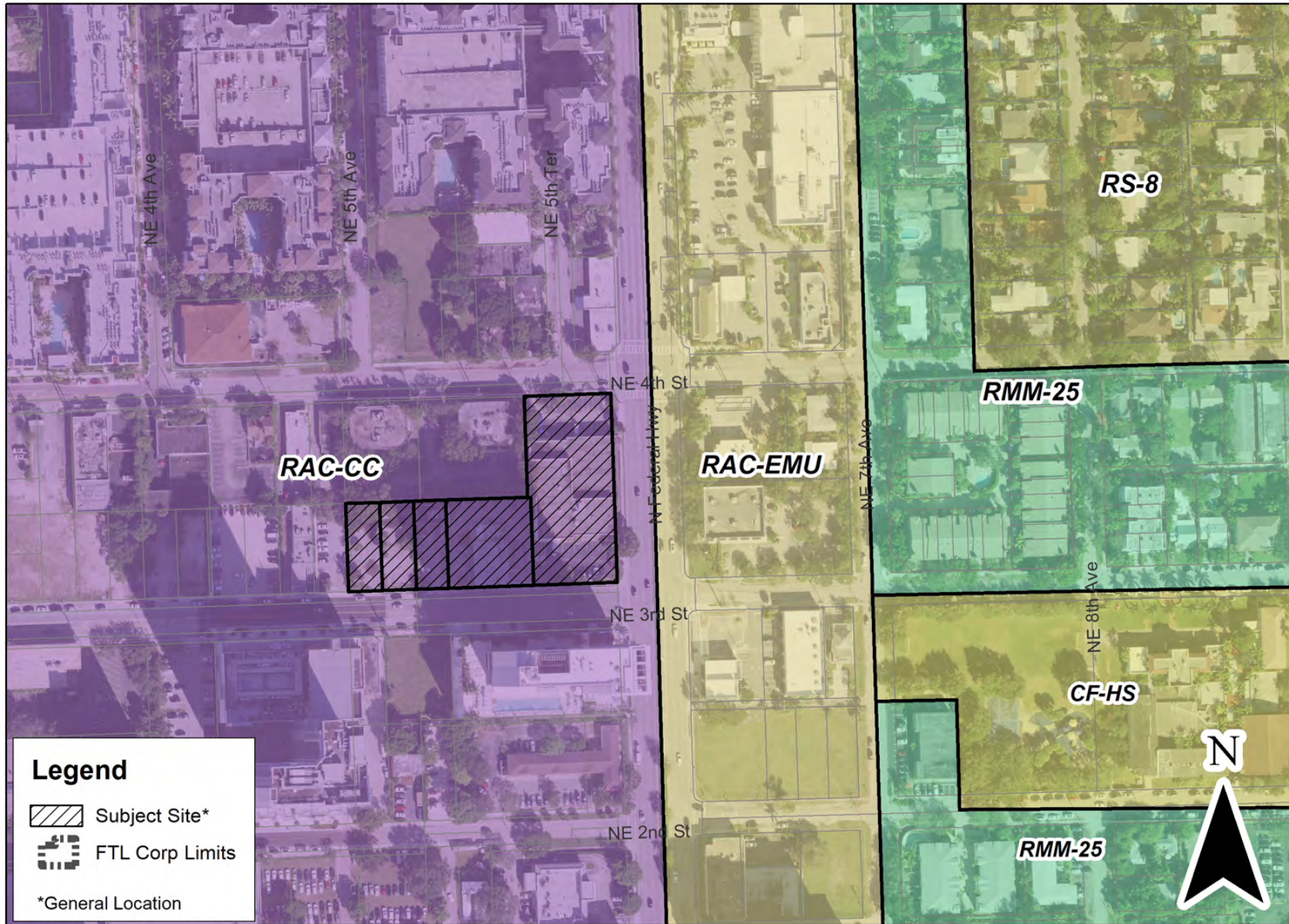
Q5	Parking Podium Façades: Where structured parking must be exposed to the street, exceptionally creative solutions should be explored Comment: Refer to DRC comment		X		
Q6	Response to Natural Environment: Encourage architecture to respond to the unique nature of the south Florida environment (solar orientation, wind direction, rain). Examples: Open breezeway corridors oriented toward prevailing winds; energy efficient glazing; above ground stormwater capture and re-use through bio-swales and rain gardens; solar roof panes/awnings.	X			
Q7	Creative Façade Composition: Encourage a rich layering of architectural elements throughout the building, with special attention to facades below the shoulder level. Comment: Refer to DRC comment		X		
Q8	Original, Self-Confident Design: Encourage a range of architectural styles that each create a strong identity, strive for the highest quality expression of its chosen architectural vocabulary. Comment: Refer to DRC comment		X		
STOREFRONTS		YES	NO	N/A	MORE INFORMATION NEEDED
SF1	Retail Location Strategy: Encourage ground floor retail in preferred locations.	X			
SF2	Encourage a combination of storefront styles and types in adjacent buildings, or within single buildings, to create variety and visual interest at the street level.		X		
SF3	Encourage durable materials for ground floor retail and cultural uses.				X
SF4	Encourage 15 foot minimum floor-to-floor height and encourage interior ground floor flush with adjacent public sidewalk. Comment: Not at grade – ramp proposed.	X			
SF5	Encourage significant glass coverage for transparency and views. Encourage restaurants to provide clear visual and physical connections to outdoor seating.				X
SF6	Encourage pedestrian shading devices of various types (min 5 foot depth).				X
SF7	Encourage multi-level storefront displays to disguise unfriendly uses or blank walls				X
SF8	Encourage well-designed night lighting solutions				X

INSTRUCTIONS: Choose applicable character area:

CHARACTER AREA (APPLICABLE AREA: DOWNTOWN CORE)		YES	NO	N/A	MORE INFORMATION NEEDED
1A *ULDR*	Frame street with appropriate streetwall height: Shoulder: 9 floors max	X			
1B *ULDR*	Signature Tower: Special architectural design encouraged for buildings over 37 floors.				X
1C *ULDR*	Encourage slender towers to complement the skyline and provide more light and air to streets and open spaces below based on maximum floorplate size. -Non-residential: 32,000 GSF floorplate max. -Residential: Buildings up to 15 floors: 18,000 GSF floorplate max. -Residential: Buildings over 15 floors: 12,500 GSF floorplate max.	X			
TOD GUIDELINES (GENERAL APPLICABILITY)		YES	NO	N/A	MORE INFORMATION NEEDED
T2	Discourage land uses that are incompatible with transit and walkability.				X
T3	Encourage pedestrian connections to transit stops and bike parking.				X
T4	Encourage bike connections to transit stops and bike parking.				X
T5	Parking consistent with TOD Principles: Encourage structured parking with screening or liner building if parking provided.				X



	Surface parking should be configured into smaller lots rather than one large lot. Surface parking discouraged except pick up/drop off within 200 feet of a Gateway Hub transit station. Parking should not face onto plaza or park space of any transit station. Include parking for mopeds, scooters, motorcycles, and other similar vehicles				
T6	Incorporate Transportation Demand Management (TDM) including but not limited to Encourage carpooling or vanpooling. Encourage car or bike sharing. Offer flexible hours. Provide shared parking.				X
T7	Reduce parking to eliminate excess pavement and promote highest and best use of land within the station area.				X
T8	Encourage green buildings, green site design and green infrastructure.				X
T9	Create attractive, active and safe multimodal systems.				X



UDP-S21003

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